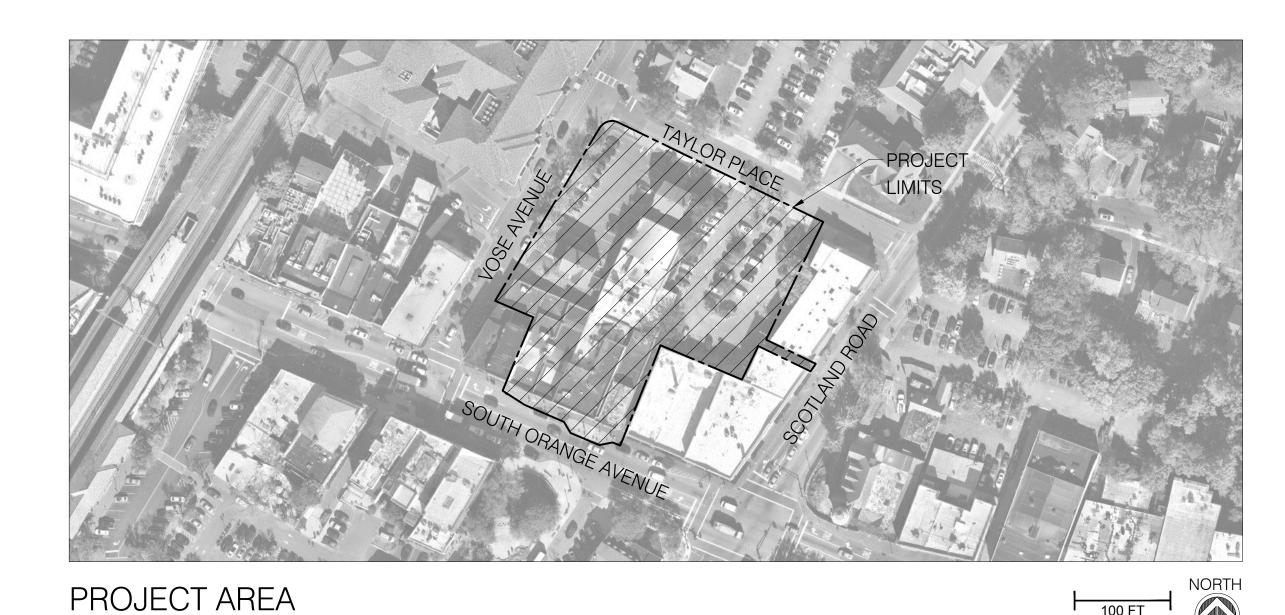
## PRELIMINARY AND FINAL SITE PLAN FOR:

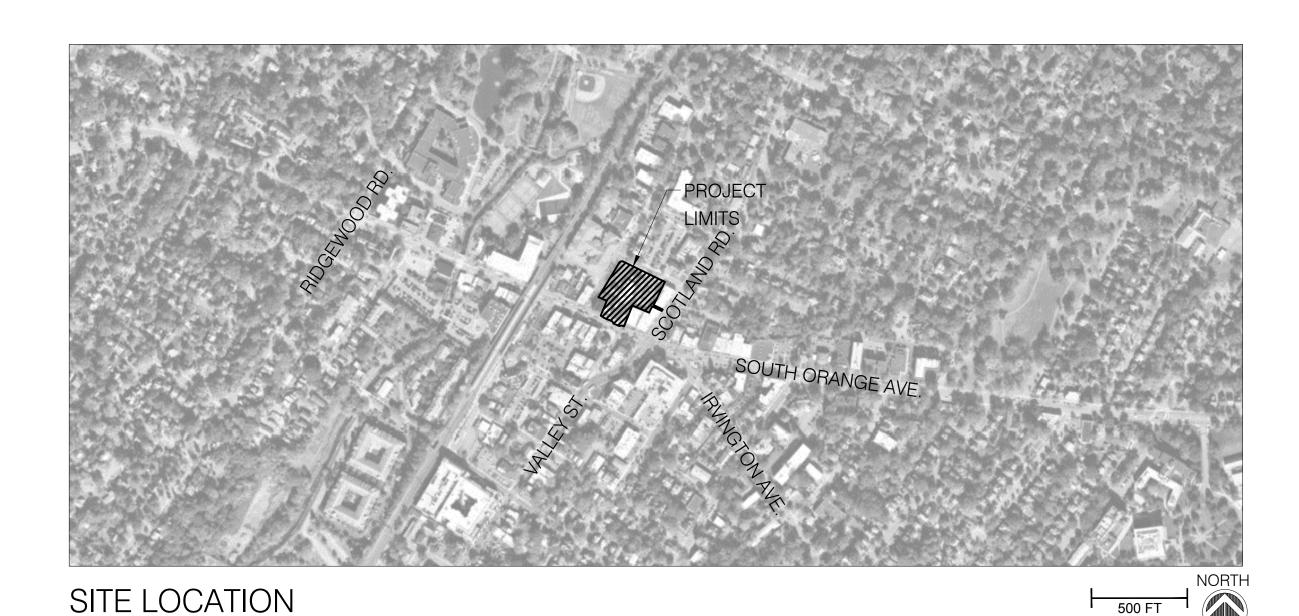
# TAYLOR & VOSE DEVELOPMENT

Block: 1006, Lots: 1,2,3,9,10,11,13,14

Village of South Orange. Essex County. New Jersey

July 7, 2020





\* THE CONTRACTOR SHALL CONTACT NJ ONE CALL AT 1-800-272-1000 PRIOR TO THE START OF CONSTRUCTION. ALL EXISTING UTILITIES SHALL BE MARKED OUT BY THE RESPECTIVE UTILITY COMPANIES PRIOR TO ANY EXCAVATION WORK.

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## PREPARED BY:

Landscape Architect:



507 Bloomfield Avenue, 2nd Floor Montclair, New Jersey 07042 USA www.ArterialStreets.com Tel / Fax 973 320 9123 info@arterialstreets.com

## PREPARED FOR:

VOSE AVE. APTS. URBAN RENEWAL, LLC. 447 Northfield Avenue, Suite 200 West Orange, NJ 07052 TAYLOR & VOSE DEVELOPMENT South Orange, NJ

PRELIMINARY AND FINAL SITE PLAN SUBMISSION

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No.	Date	Remarks
1	08/06/2020	Updated Preliminary & Final Site Plan Se
2	08/21/2020	Updated Preliminary & Final Site Plan Se
Sheet	Title:	

COVER	SHEET
By: VH	
ed By:	-
DL	
07/07/2020	
Number:	Sheet:
02-2020-130	1 of 11

- 1. THE INFORMATION SHOWN CONCERNING THE TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING THEIR OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.
- 2. THE CONTRACTOR SHALL CONTACT NJ ONE CALL AT 1-800-272-1000 PRIOR TO THE START OF CONSTRUCTION. ALL EXISTING UTILITIES SHALL BE MARKED OUT BY THE RESPECTIVE UTILITY COMPANIES PRIOR TO ANY EXCAVATION WORK.
- 3. ANY DAMAGE TO UTILITIES SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. ALL COST FOR REPAIRS SHALL BE BORNE BY THE CONTRACTOR.
- 4. ALL CONSTRUCTION SHOWN HEREIN, UNLESS SPECIFICALLY NOTED OTHERWISE, SHALL BE IN ACCORDANCE WITH THE NEW JERSEY DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2007 AS AMENDED.
- 5. ALL PROPERTY CORNERS OR MONUMENTS REMOVED DURING CONSTRUCTION SHALL BE REPLACED BY A NEW JERSEY LICENSED LAND SURVEYOR AT NO ADDITIONAL COST TO THE OWNER.
- 6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DISPOSAL OF THE EXCESS MATERIALS EXCAVATED OF WHATEVER NATURE AT THEIR OWN EXPENSE. THE OWNER IS NOT OBLIGATED TO SUPPLY A DISPOSAL SITE.
- 7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ARRANGING OF STORAGE AND STAGING AREAS AT NO ADDITIONAL COST TO THE OWNER.
- 8. APPROPRIATE CONSTRUCTION SIGNING SHALL BE INSTALLED FIVE (5) DAYS PRIOR TO THE START OF CONSTRUCTION. SIGNAGE SHALL BE MAINTAINED DURING CONSTRUCTION AND SHALL BE BAGGED DURING PERIODS OF INACTIVITY.
- 9. ALL UTILITY STRUCTURES (UTILITY BOXES, OIL FILL CAPS, WATER VALVES, GAS VALVES, BILLCO DOORS, ELECTRICAL VAULTS AND ANY OTHER APPURTENANCES) WITHIN THE PROPOSED SIDEWALK RECONSTRUCTION SHALL BE RESET TO CONFORM TO THE PROPOSED FINISHED GRADE.
- 10. CONTRACTOR IS RESPONSIBLE FOR SURVEYING AND STAKING THE PROPOSED CENTERLINE STATIONING AND ALL PROPOSED ELEMENTS AT NO ADDITIONAL COST TO THE OWNER. THE COST FOR THIS SHOULD BE BUILT INTO THE RELATED ITEMS.
- 11. CONTRACTOR'S FIELD STAKEOUT SURVEYOR IS RESPONSIBLE FOR VERIFYING EXISTING SITE CONDITIONS AND NOTING ANY DISCREPANCIES BETWEEN SITE FIELD CONDITIONS AND STAKING PLAN PRIOR TO CONSTRUCTION. SHOULD ANY DISCREPANCIES EXIST, CONTRACTOR'S SURVEYOR SHALL NOTIFY OWNER IN WRITING PRIOR TO COMMENCEMENT OF WORK. TOWNSHIP AND CONSULTANT TEAM ASSUME NO RESPONSIBILITY FOR WORK PERFORMED PRIOR TO FIELD VERIFICATION OF STAKING PLAN.
- 12. THIS PLAN IS A REPRESENTATION OF DESIGN INTENT ONLY AND AS SUCH DENOTES VERTICAL AND HORIZONTAL RELATIONSHIPS, MATERIALS AND FINISHES. CONTRACTOR SHALL SUBMIT TO THE LANDSCAPE ARCHITECT SHOP DRAWINGS INDICATING STRUCTURAL DESIGN, AS WELL AS PLANS AND ELEVATIONS, APPROPRIATE TO ENSURE DESIGN INTENT IS MET.
- 13. CONTRACTOR SHALL TAKE CARE TO PROTECT FROM CONSTRUCTION ALL EXISTING TREES, STRUCTURES UNDER/ABOVE GROUND SURFACE THAT ARE TO REMAIN. ANY DAMAGE DONE TO THESE DURING CONSTRUCTION MUST BE REPLACED AT CONTRACTOR'S OWN COST WITHIN THE CONSTRUCTION PERIOD.
- 14. CONTRACTOR SHALL OBTAIN ALL PERMITS NECESSARY FOR CONSTRUCTION FROM THE APPROPRIATE LOCAL, STATE AND FEDERAL AGENCIES.
- 15. CONTRACTOR SHALL LEAVE THE SITE EACH DAY IN AN ORDERLY FASHION AND SHOULD REMOVE ALL LITTER, REFUSE, DEBRIS AND HAZARDOUS MATERIALS FROM THE SITE DAILY, IN COMPLIANCE WITH APPROPRIATE LOCAL, STATE, AND FEDERAL REGULATIONS. CONTRACTOR SHALL TAKE MEASURES TO LIMIT THE AMOUNT OF SOIL LEAVING THE SITE ON EQUIPMENT. ANY DEBRIS ON ROAD SURFACES OR SIDEWALK SHOULD BE CLEANED DAILY.
- 16. IF APPLICABLE, THE CONTRACTOR MUST PROVIDE A PORTABLE RESTROOM UNIT FOR THE DURATION OF THE PROJECT IN A LOCATION APPROVED BY THE OWNER.
- 17. CONTRACTOR SHALL BE REQUIRED TO SAFELY SECURE THE SITE THROUGHOUT THE DURATION OF CONSTRUCTION AND FINAL APPROVAL. ALL COSTS ASSOCIATED WITH SECURING THE SITE, INCLUDING TEMPORARY FENCING, SHALL BE INCLUDED IN THE VARIOUS ITEMS ABOVE. ANY DAMAGE OF VANDALISM (SUCH AS FOOTSTEPS/HAND PRINTS IN SIDEWALK, BROKEN AND DAMAGED FURNITURE, ETC.) THAT OCCURS DURING THE CONSTRUCTION PERIOD WILL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND CONSIDERED DUE TO LACK OF SECURED SITE. ALL COSTS ASSOCIATED WITH WORK TO REMEDY THE DAMAGE SHALL BE BORNE BY THE CONTRACTOR.
- 18. ALL DISTURBED SHRUBS, FENCING, SIGNS, MAIL BOXES, DRIVES, ETC. SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND TO THE SATISFACTION OF THE OWNER. NO SEPARATE PAYMENT SHALL BE MADE FOR THIS RESTORATION, UNLESS SPECIFIED ELSEWHERE.
- 19. ALL STREET LIGHTS AND LIGHT BOLLARDS SHALL TIE INTO TOWNSHIP STREET LIGHT METER.

#### MATERIALS & LAYOUT NOTES

- 1. ALL CONSTRUCTION SHOWN HEREIN, UNLESS SPECIFICALLY NOTED OTHERWISE, SHALL BE IN ACCORDANCE WITH THE NEW JERSEY DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2007 AS AMENDED.
- CONTRACTOR SHALL LAYOUT AND STAKE IN THE FIELD THE ALIGNMENT OF ALL CURBS, PAVEMENTS AND OTHER IMPROVEMENTS FOR REVIEW BY THE OWNER. CONTRACTOR MAY NOT PROCEED WITH CONSTRUCTION OF IMPROVEMENTS UNTIL FINAL APPROVAL OF THE LAYOUT IS GRANTED BY THE OWNER.
- 3. THE CONTRACTOR SHALL VERIFY ALL EXISTING SITE CONDITIONS, BOTH ABOVE AND BELOW THE SURFACE. ANY DISCREPANCIES BETWEEN INFORMATION SHOWN ON THE DRAWINGS AND ACTUAL FIELD CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER IN WRITING, PRIOR TO COMMENCING WORK. DO NOT WILLFULLY PROCEED WITH WORK IF ANY DISCREPANCIES ARE DISCOVERED, OTHERWISE. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE BORNE BY THE CONTRACTOR.
- CONTRACTOR SHALL BE RESPONSIBLE FOR MAKING HIMSELF FAMILIAR WITH ALL UNDERGROUND UTILITIES AND STRUCTURES. THE CONTRACTOR SHALL TAKE SOLE RESPONSIBILITY FOR ANY COST INCURRED DUE TO DAMAGE OF SAID UTILITIES OR STRUCTURES.
- 5. NO CHANGES SHALL BE MADE TO THE DESIGN OR LAYOUT WITHOUT THE WRITTEN APPROVAL OF THE OWNER. LAYOUT THE WORK AS DIMENSIONED OR STATIONED ON THE PLANS. WRITTEN DIMENSIONS SHALL GOVERN. DO NOT SCALE DISTANCES.
- 6. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA).
  - 6.1 ALL CURB RAMPS SHALL BE ADA COMPLIANT.
  - 6.2 SPECIAL CARE SHOULD BE TAKEN TO ENSURE THAT CURB RAMPS ARE NOT LOCATED AT LOW POINTS. PAVEMENT AT THE BASE OF THE CURB RAMP SHALL BE SLOPED HORIZONTALLY AT 1% MINIMUM AWAY FROM THE CURB RAMP AND TOWARDS INLETS. THIS IS TO ENSURE THAT PUDDLING IS AVOIDED AT CURB RAMPS.
  - 6.3 CURB RAMPS AND SIDEWALKS THAT ARE NOT CONSTRUCTED IN COMPLIANCE WITH ADA ACCESSIBILITY REQUIREMENTS INCLUDING MAXIMUM RUNNING SLOPES, MAXIMUM CROSS SLOPES, LANDING AREA LOCATION AND SIZE, DIVERGING SIDEWALK DROP OFF PROTECTION, BLENDED TRANSITIONS, BLENDED TRANSITION RUNNING SLOPES AND CROSS SLOPES, FLARE WIDTHS, GRADE BREAKS AND GRADE BREAK DIRECTIONS, CLEAR SPACE SIZE AND LOCATION, WILL NOT BE ACCEPTED.
  - 6.4 BIDS THAT TAKE ADVANTAGE OF ANY ERRORS AND/OR OMISSIONS IN THE CONTRACT DRAWINGS OR DISCREPANCIES WITH THE SPECIAL PROVISIONS WILL NOT BE ACCEPTED. IN THE EVENT ANY SUCH ERROR, OMISSION, OR DISCREPANCIES ARE DISCOVERED, IMMEDIATELY NOTIFY THE ENGINEER. FAILURE TO NOTIFY THE ENGINEER WILL CONSTITUTE A WAIVER OF ALL CLAIMS FOR MISUNDERSTANDINGS, AMBIGUITIES, OR OTHER SITUATIONS RESULTING FROM THE ERROR, OMISSION, OR DISCREPANCY.
  - 6.5 ALL MINIMUM AND MAXIMUM DIMENSIONS CONTAINED IN THE CONTRACT DRAWINGS ARE ABSOLUTE. ACCEPTANCE WILL NOT BE ISSUED IF ANY NEWLY CONSTRUCTED RAMP DOES NOT COMPLY WITH THE APPROVED DRAWINGS. CONSTRUCTION STANDARDS AND SPECIFICATIONS.
  - 6.6 ALTERATIONS TO EXISTING FACILITIES MUST MEET THE ADA REQUIREMENTS TO THE MAXIMUM EXTENT FEASIBLE. FOR ALTERATIONS WHERE IT IS TECHNICALLY INFEASIBLE TO MEET THE ADA REQUIREMENTS, A TECHNICALLY INFEASIBLE DOCUMENT MUST BE SUBMITTED TO THE TOWNSHIP TO DOCUMENT THAT ACCESS HAS BEEN PROVIDED TO THE MAXIMUM EXTENT FEASIBLE. DO NOT BEGIN CONSTRUCTION OF THE CURB RAMP OR DRIVEWAY CROSSING UNTIL APPROVAL OF TECHNICAL INFEASIBLE DOCUMENT IS OBTAINED FROM THE OWNER.
- 7. ALL SITE FURNITURE LOCATIONS SHALL BE APPROVED IN THE FIELD BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- 8. ALL SIDEWALKS SHALL HAVE A RUNNING SLOPE OF NO GREATER THAN 5% AND A CROSS SLOPE NO GREATER THAN 2% (PER ADAAG) UNLESS NOTED OTHERWISE ON GRADING PLAN.
- 9. ALL NEW CURBS AND PAVEMENTS SHALL MEET EXISTING CURBS & PAVEMENTS SMOOTH AND FLUSH.
- 10. NEW CURBS, WALLS, AND PAVEMENTS SHALL BE BUILT TO A SMOOTH EVEN FINISH WITH A CONSISTENT TOP AND PROFILE WITHOUT WAVES OR IRREGULARITIES. ANY WORK NOT MEETING THIS QUALITY STANDARD SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- 11. DO NOT WILLFULLY PROCEED WITH CONSTRUCTION AS DESIGNED WHEN OBSTRUCTIONS AND /OR GRADE DIFFERENCES EXISTS THAT MAY NOT HAVE BEEN KNOWN DURING DESIGN. DO NOT PROCEED WITH CONSTRUCTION AND ORDERING MATERIALS WHEN INFORMATION IS INCOMPLETE OR OTHER DISCREPANCIES MAY EXIST. CONTRACTOR SHALL IMMEDIATELY NOTIFY THE TOWNSHIP OF SUCH CONDITIONS. THE CONTRACTOR SHALL ASSUME COMPLETE RESPONSIBILITY FOR ALL NECESSARY REVISIONS DUE TO FAILURE TO GIVE SUCH NOTIFICATION.
- 12. CONTRACTOR IS RESPONSIBLE FOR POWER AND WATER NEEDED DURING CONSTRUCTION.

#### PLANTING NOTES

- 1. DO NOT WILLFULLY PROCEED WITH PLANTING OPERATIONS AS DESIGNED WHEN IT IS OBVIOUS THAT UNKNOWN OBSTRUCTIONS AND/OR GRADE DIFFERENCES EXIST THAT MAY NOT HAVE BEEN KNOWN DURING THE DESIGN PROCESS. SUCH CONDITIONS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT. THE CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY FOR ALL NECESSARY REVISIONS DUE TO FAILURE TO GIVE SUCH NOTIFICATION.
- 2. ALL TREES SHALL BE TAGGED BY THE LANDSCAPE ARCHITECT AT A NURSERY SELECTED BY THE CONTRACTOR
- 3. IF CONFLICTS ARISE BETWEEN THE ACTUAL SIZE OF PLANTING AREAS ON THE SITE AND THE DRAWINGS, CONTACT THE LANDSCAPE ARCHITECT FOR RESOLUTION.
- 4. GROUNDCOVERS AND SHRUBS ARE TO BE TRIANGULARLY SPACED UNLESS OTHERWISE INDICATED ON THE DRAWINGS
- 5. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FURNISH PLANT MATERIAL FREE OF PESTS OR PLANT DISEASES. PRE-SELECTED OR "LANDSCAPE ARCHITECT TAGGED" PLANT MATERIAL MUST BE INSPECTED BY THE CONTRACTOR AND CERTIFIED PEST AND DISEASE FREE. IT IS THE CONTRACTOR'S OBLIGATION TO WARRANTY ALL PLANT MATERIAL PER THE SPECIFICATIONS.
- 6. THE ACTUAL LOCATION OF PLANT MATERIAL MAY VARY DUE TO FIELD CONDITIONS. FINAL PLACEMENT OF PLANT MATERIAL SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT BEFORE PITS ARE DUG. NO PLANTS SHALL BE PUT INTO GROUND BEFORE ROUGH GRADING HAS BEEN FINISHED AND APPROVED BY THE OWNER.
- 7. ALL PLANTS SHALL BE WATERED THOROUGHLY TWICE DURING THE 24-HOUR PERIOD AFTER PLANTING. ALL PLANTS SHALL BE WATERED WEEKLY OR MORE OFTEN DURING THE FIRST GROWING SEASON. THE CONTRACTOR SHALL REFER TO CONTRACT SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
- 8. ANY PLANT DEEMED NOT AVAILABLE BY THE CONTRACTOR SHALL BE NOTED IN THE BID AS A CONDITION OF BID. FAILURE TO QUALIFY AVAILABILITY OF SPECIFIED MATERIAL SHALL MAKE THE CONTRACTOR RESPONSIBLE FOR SUPPLYING ALL MATERIALS AS SPECIFIED.

### LEGEND

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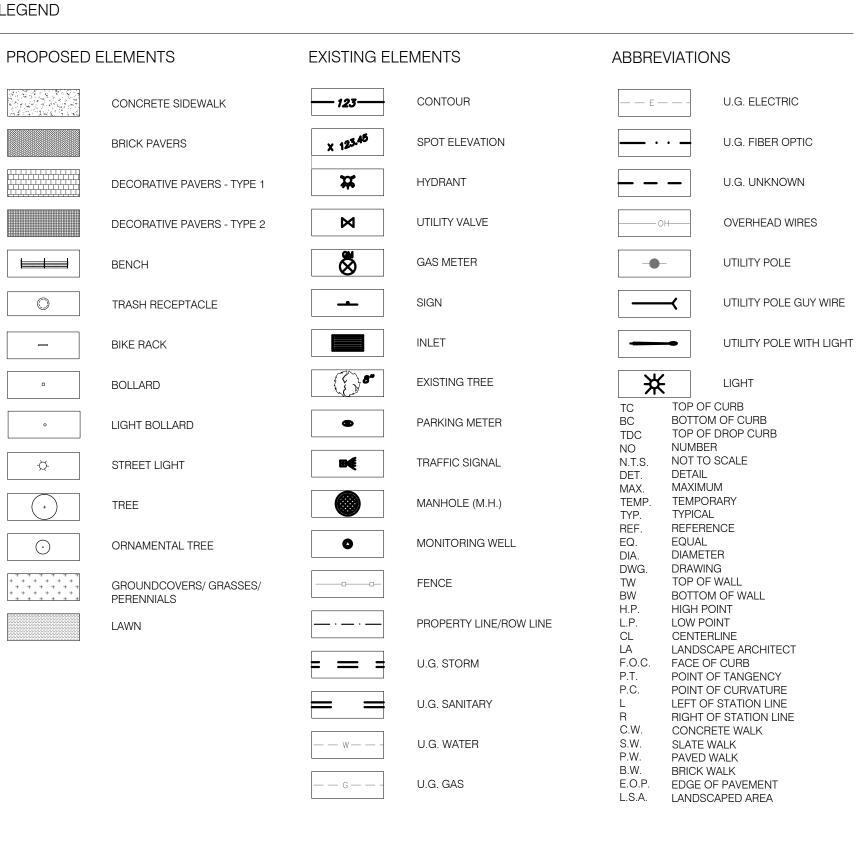
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PRELIMINARY AND FINAL SITE PLAN SUBMISSION

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Landscape Architect



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NOT FOR CONSTRUCTION

QQ_
David I. Lustberg

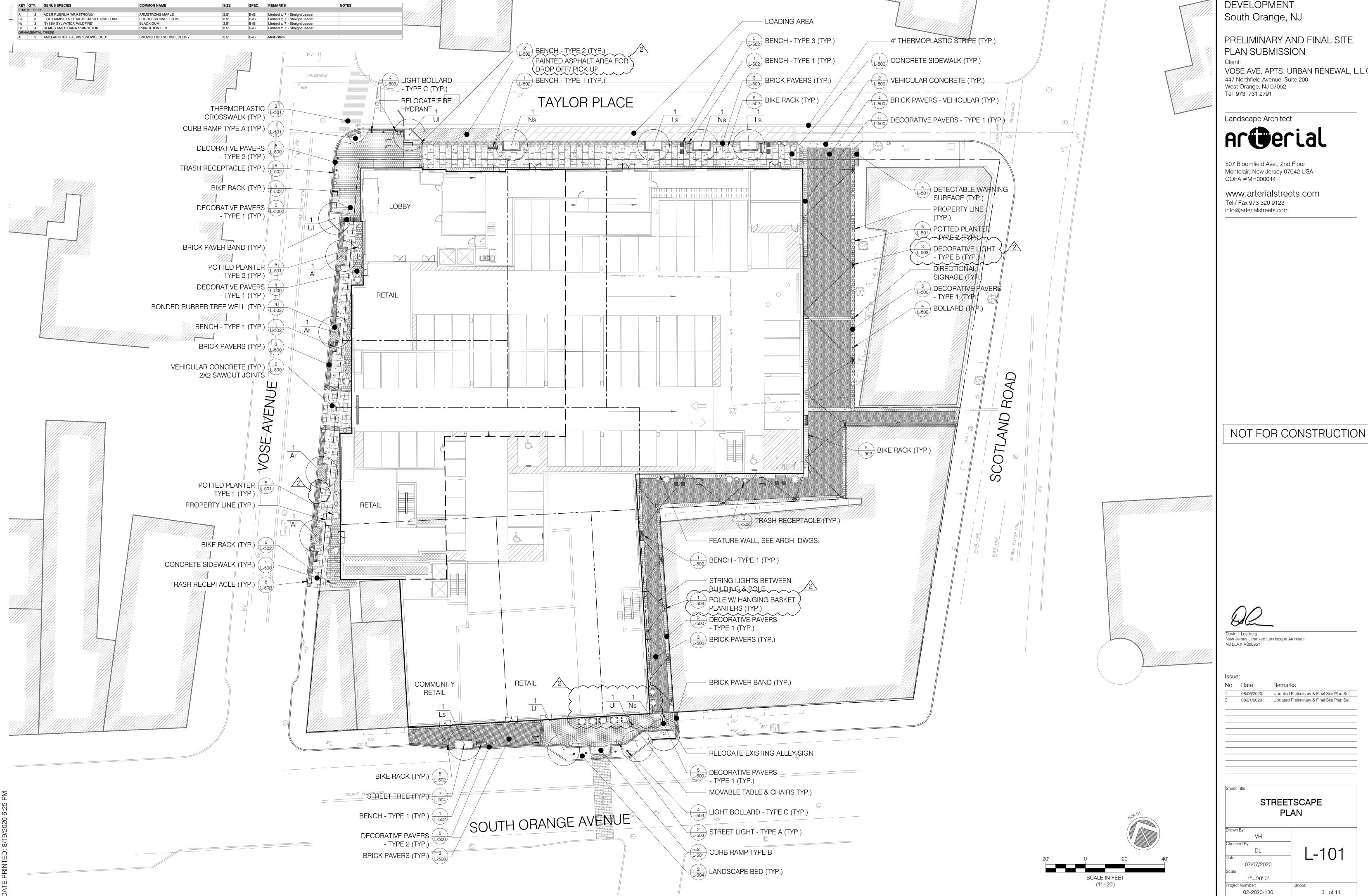
New Jersey Licensed Landscape Architect

Issue: No. Date

08/06/2020 Updated Preliminary & Final Site Plan Set 08/21/2020 Updated Preliminary & Final Site Plan Set

NOTES. LEGEND & SCHEDULE Drawn By:

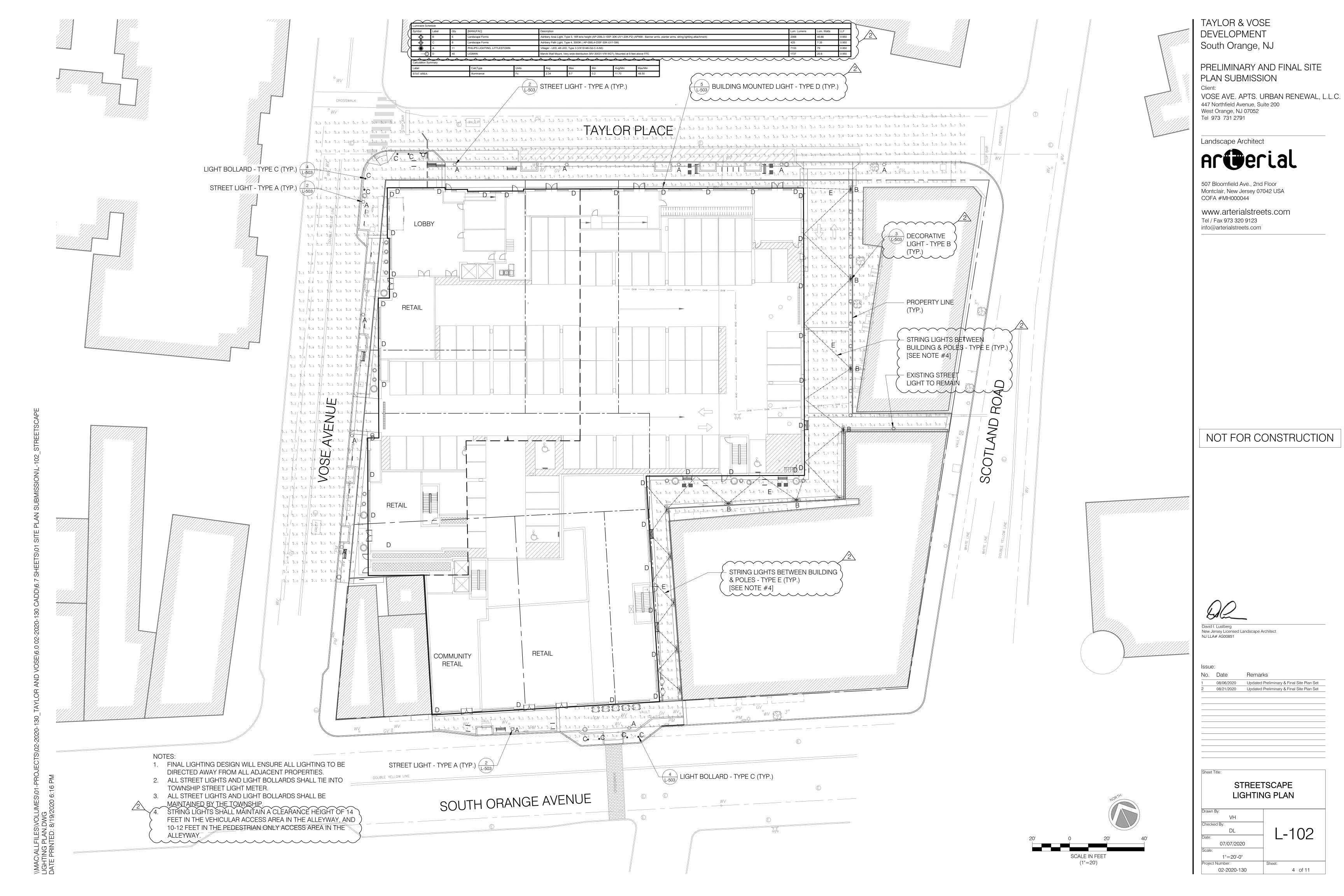
VH Checked By: DL \_-001 07/07/2020 N/A 02-2020-130 2 of 11



PLANT SCHEDULE

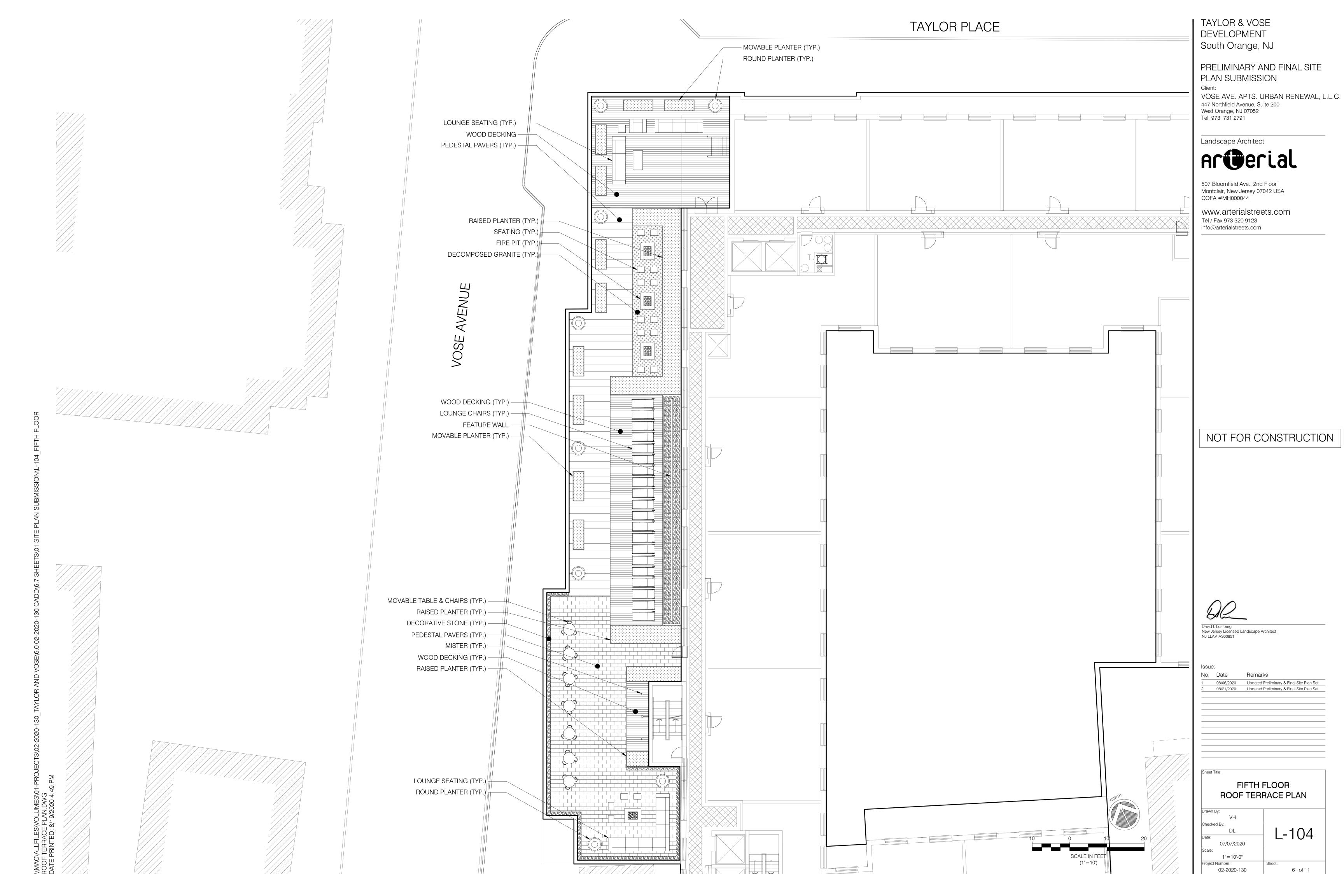
TAYLOR & VOSE DEVELOPMENT

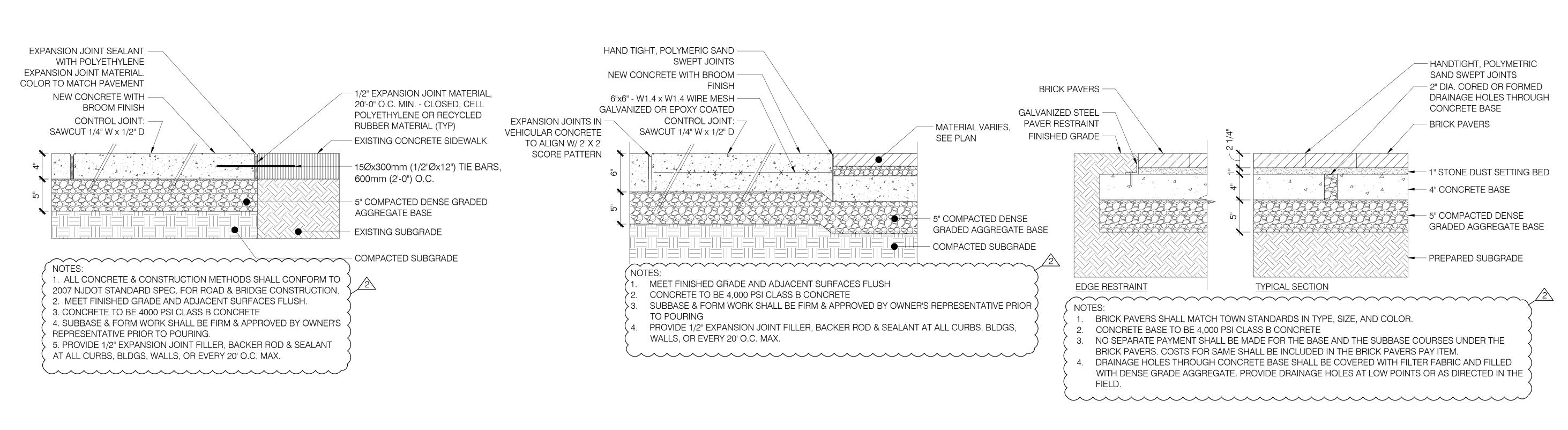
VOSE AVE. APTS. URBAN RENEWAL, L.L.C.



11-PROJECTS\02-2020-130\_TAYLOR AND VOSE\6.0 02-2020-130 CADD\6.7 SHEETS\01 SITE PLAN SUBMISSION\L-103\_SECOND FLOO

SD PLAN.DWG





BRICK PAVERS

BRICK PAVERS

2" DIA. CORED OR FORMED DRAINAGE HOLES THROUGH CONCRETE BASE

PAVER RESTRAINT
FINISHED GRADE

1" STONE DUST SETTING BED

6" CONCRETE BASE

PREPARED SUBGRADE

NOTES:
1. BRICK PAVERS SHALL MATCH TOWN STANDARDS IN TYPE, SIZE, AND COLOR.
2. CONCRETE BASE TO BE 4,000 PSI CLASS B CONCRETE

**EDGE RESTRAINT** 

SCALE: 1-1/2" = 1'-0"

CONCRETE SIDEWALK

3. NO SEPARATE PAYMENT SHALL BE MADE FOR THE BASE AND THE SUBBASE COURSES UNDER THE BRICK PAVERS. COSTS FOR SAME SHALL BE INCLUDED IN THE BRICK PAVERS PAY ITEM.

TYPICAL SECTION

4. DRAINAGE HOLES THROUGH CONCRETE BASE SHALL BE COVERED WITH FILTER FABRIC AND FILLED WITH DENSE GRADE AGGREGATE. PROVIDE DRAINAGE HOLES AT LOW POINTS OR AS DIRECTED IN THE FIELD.

2 VEHICULAR CONCRETE

SCALE: 1-1/2" = 1'-0"

3 BRICK PAVERS

SCALE: 1-1/2" = 1'-0"

HAND TIGHT, POLYMERIC SAND SWEPT JOINTS -HAND TIGHT, POLYMERIC SAND SWEPT JOINTS RECLAIMED COBBLESTONE PAVERS - PAVERS CONCRETE (L-500) FLUSH SIDEWALK - 1-1/2" MORTAR SETTING BED — 1" MORTAR SETTING BED - 4" CONCRETE BASE - 4" CONCRETE - 5" COMPACTED DENSE GRADED AGGREGATE BASE 5" THICK COMPACTED DGA BASE COURSE - COMPACTED SUBGRADE - COMPACTED SUBGRADE-COMPACT TO 95% MODIFIED PROCTOR GRANITE PAVER SIZE AND COLOR T.B.D. CONCRETE BASE TO BE 4,000 PSI CLASS B CONCRETE COBBLESTONE PAVERS ARE TO BE RECLAIMED SIZE: 4" X 8" X 4"; PATTERN: RUNNING BOND; 5 COURSES COLOR TO BE GRAY AS SUPPLIED BY STONE FARM OR APPROVED EQUAL CONCRETE BASE TO BE 4,000 PSI CLASS B CONCRETE

4

BRICK PAVERS - VEHICULAR

5 D sc.

DECORATIVE PAVERS - TYPE 1

SCALE: 1-1/2" = 1'-0"

DECORATIVE PAVERS - TYPE 2

SCALE: 1-1/2" = 1'-0"

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TAYLOR & VOSE DEVELOPMENT South Orange, NJ

PRELIMINARY AND FINAL SITE PLAN SUBMISSION

Client:

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NOT FOR CONSTRUCTION

QQ\_

NJ LLA# AS00851

David I. Lustberg
New Jersey Licensed Landscape Architect

Issue:

No. Date Remarks

1 08/06/2020 Updated Pr

2 08/21/2020 Updated Preliminary & Final Site Plan Set

Sheet Title:

CONSTRUCTION DETAILS

Drawn By:

VH

Checked By:

DL

Date:

07/07/2020

Scale:

AS NOTED

Presingt Number:

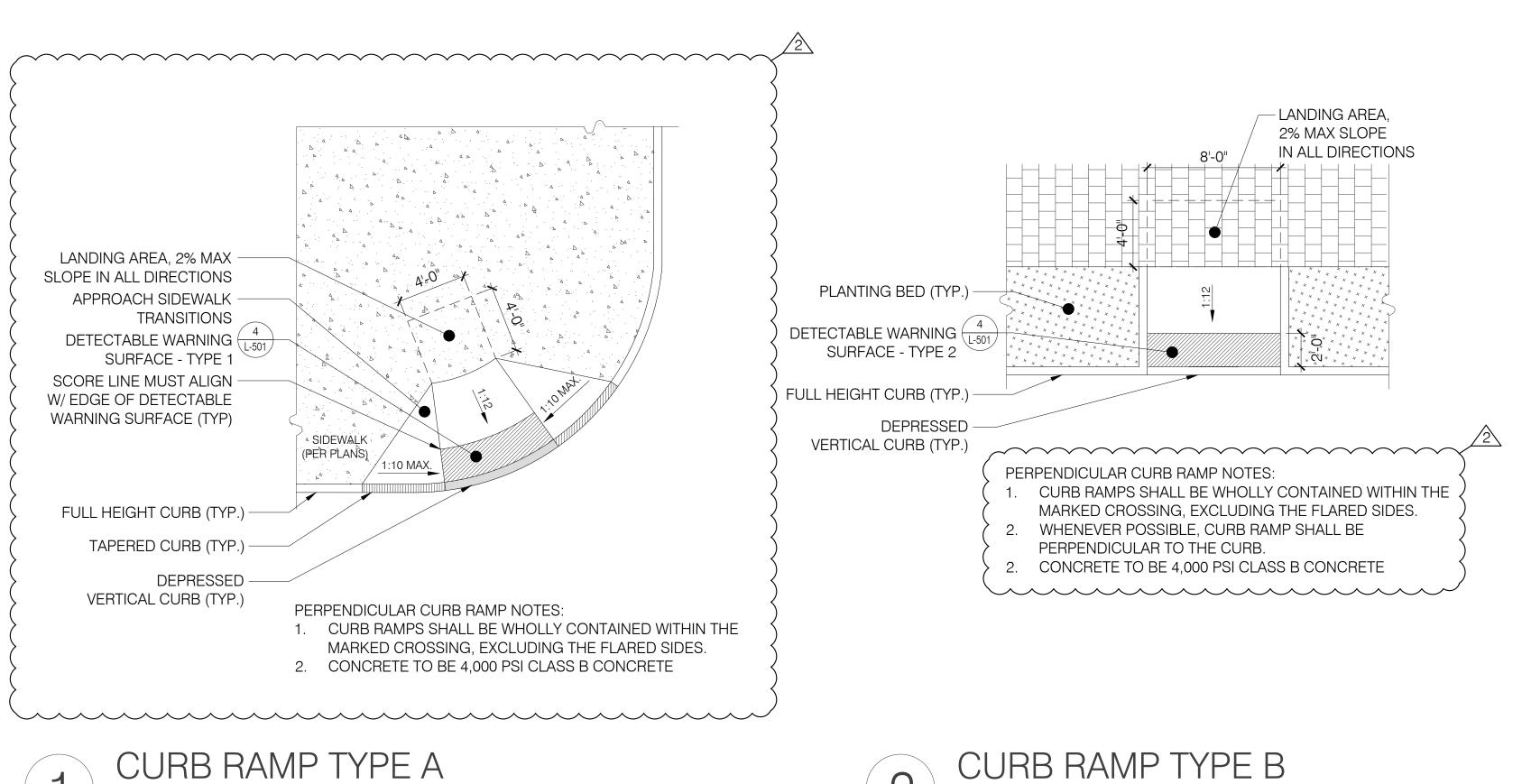
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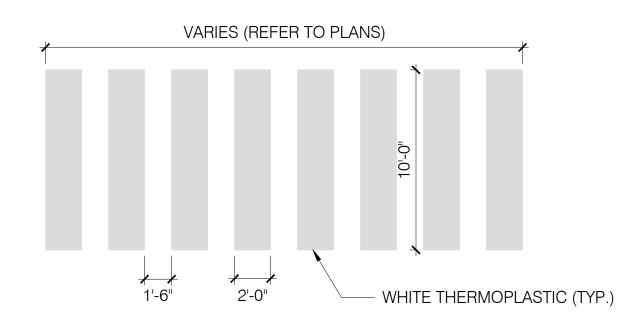
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02-2020-130

SCALE: 3/16" = 1'-0"



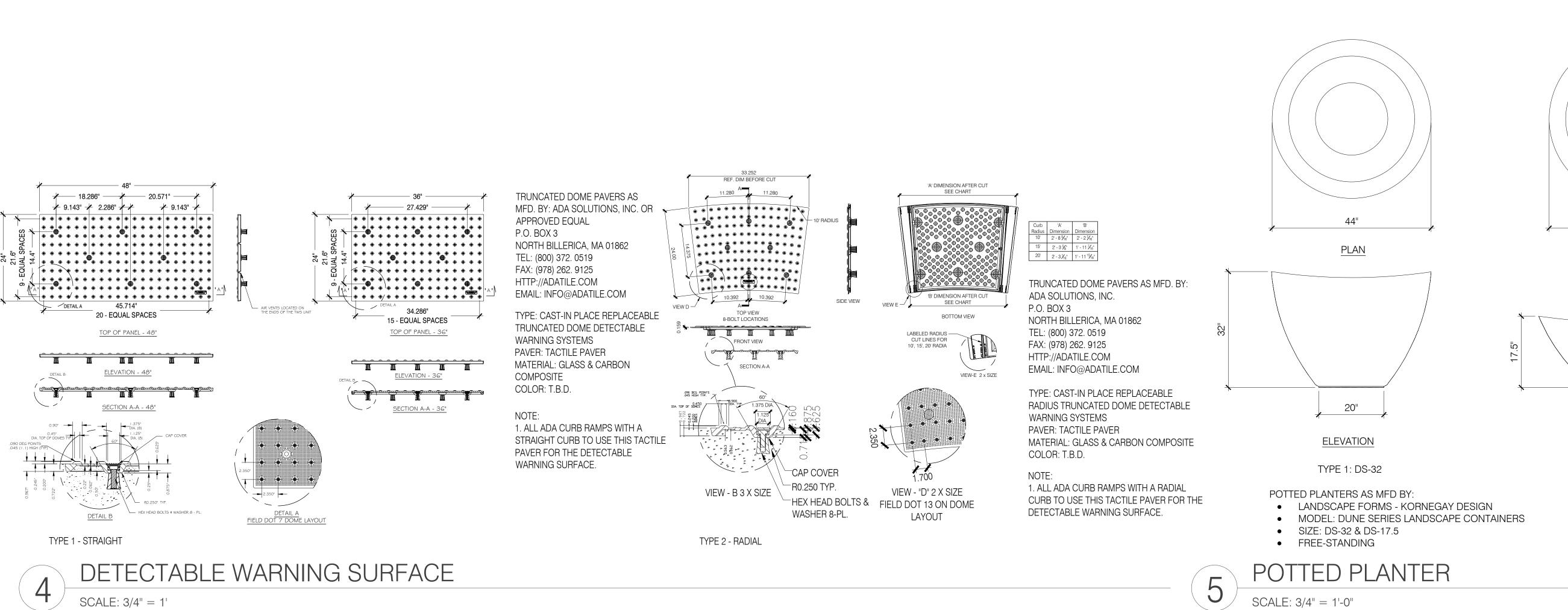




NOTES

 CURB RAMPS SHALL BE WHOLLY CONTAINED WITHIN THE MARKED CROSSING, EXCLUDING THE FLARED SIDES.





SCALE: 3/16" = 1'-0"

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**TAYLOR & VOSE** 

DEVELOPMENT

South Orange, NJ

PLAN SUBMISSION

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PRELIMINARY AND FINAL SITE

VOSE AVE. APTS. URBAN RENEWAL, L.L.C.

QQ.

<u>PLAN</u>

20"

**ELEVATION** 

TYPE 2: DS-17.5

David I. Lustberg
New Jersey Licensed Landscape Architect
NJ LLA# AS00851

Issue:

No. Date Remarks

1 08/06/2020 Updated Prelimi

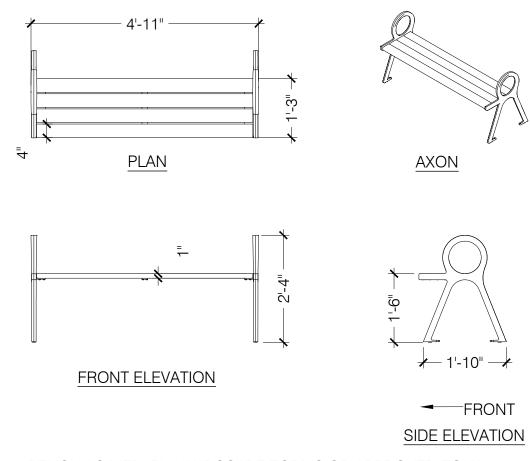
1 08/06/2020 Updated Preliminary & Final Site Plan Set
2 08/21/2020 Updated Preliminary & Final Site Plan Set

Sheet Title:

02-2020-130

CONSTRUCTION DETAILS

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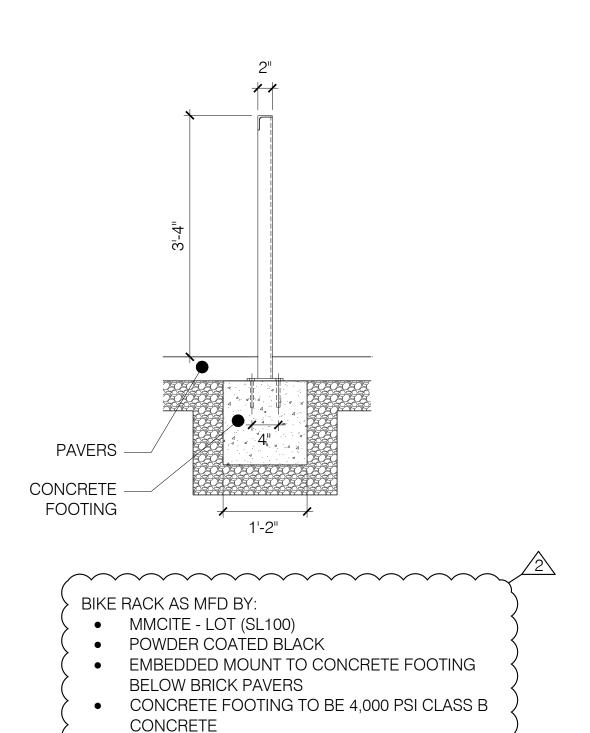
BENCH AS MFD. BY LANDSCAPE FORMS OR APPROVED EQUAL;

- MODEL: PARASPHERE BENCH
- STYLE: 59" BENCH BACKLESS
- IPE WOOD SLATS
- FRAME COLOR: MATTE BLACK



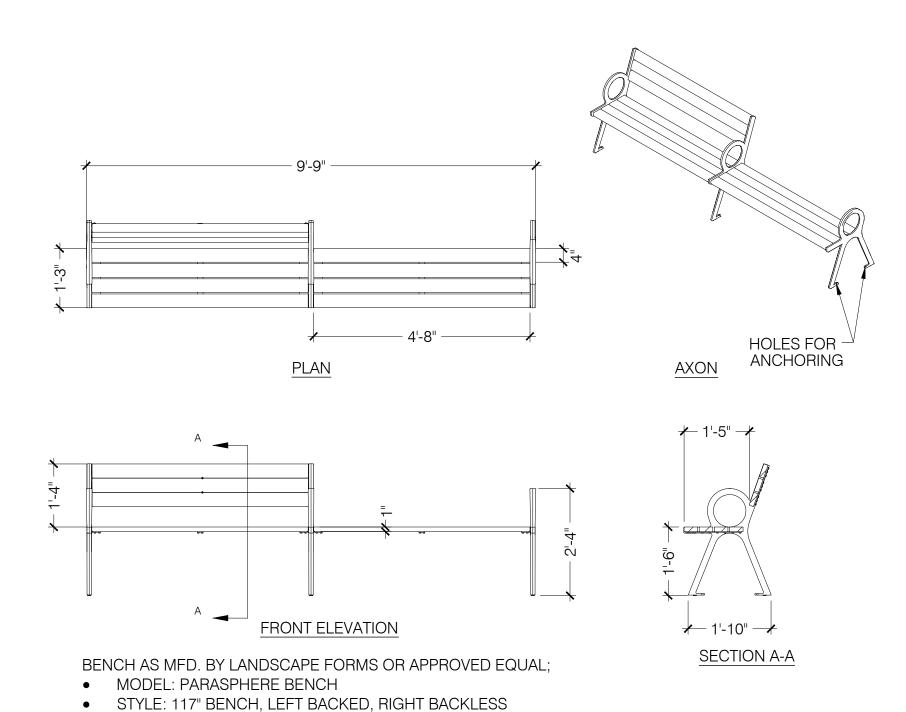
### BENCH - TYPE 1

SCALE: N.T.



BOLLARD

SCALE: 3/4" = 1'-0"



PLAN

PLAN

PLAN

1'-5"

1'-6"

ELEVATION

BENCH AS MFD. BY MMCITE OR APPROVED EQUAL;

MODEL: LRA115T RADIUM

## BENCH - TYPE 2

IPE WOOD SLATS

• FRAME COLOR: MATTE BLACK

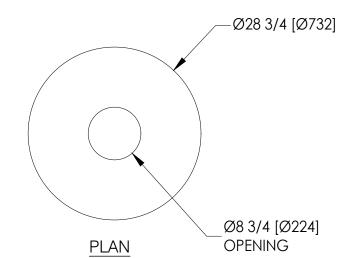
SCALE: N.T.S.

2'-4"

<u>PLAN</u>

BENCH - TYPE 3

SCALE: N.T.S.



RECEPTACLE AS MFD. BY LANDSCAPE FORMS OR

• MODEL: POE LITTER, 34

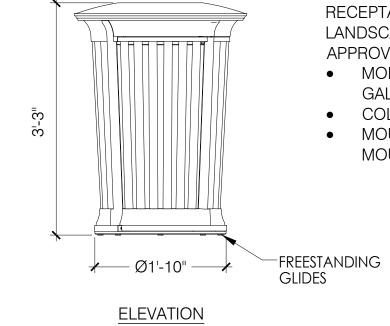
MOUNTING: SURFACE

GALLON, TOP OPENING

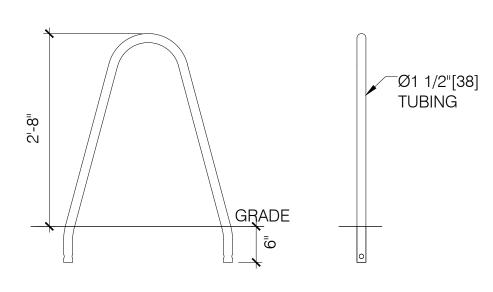
APPROVED EQUAL;

COLOR: BLACK

MOUNTED







### BIKE RACK AS MFD BY:

<u>SECTION</u>

- LANDSCAPE FORMS BOLA BIKE RACK
- POWDER COATED COLOR T.B.D.

   FMREDDED MOUNT TO CONODE
- EMBEDDED MOUNT TO CONCRETE SUBBASE BELOW PAVERS







PRELIMINARY AND FINAL SITE PLAN SUBMISSION

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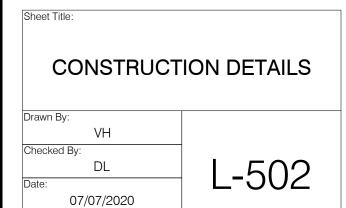
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David I. Lustberg New Jersey Licensed Landscape Architect	
NJ LLA# AS00851	

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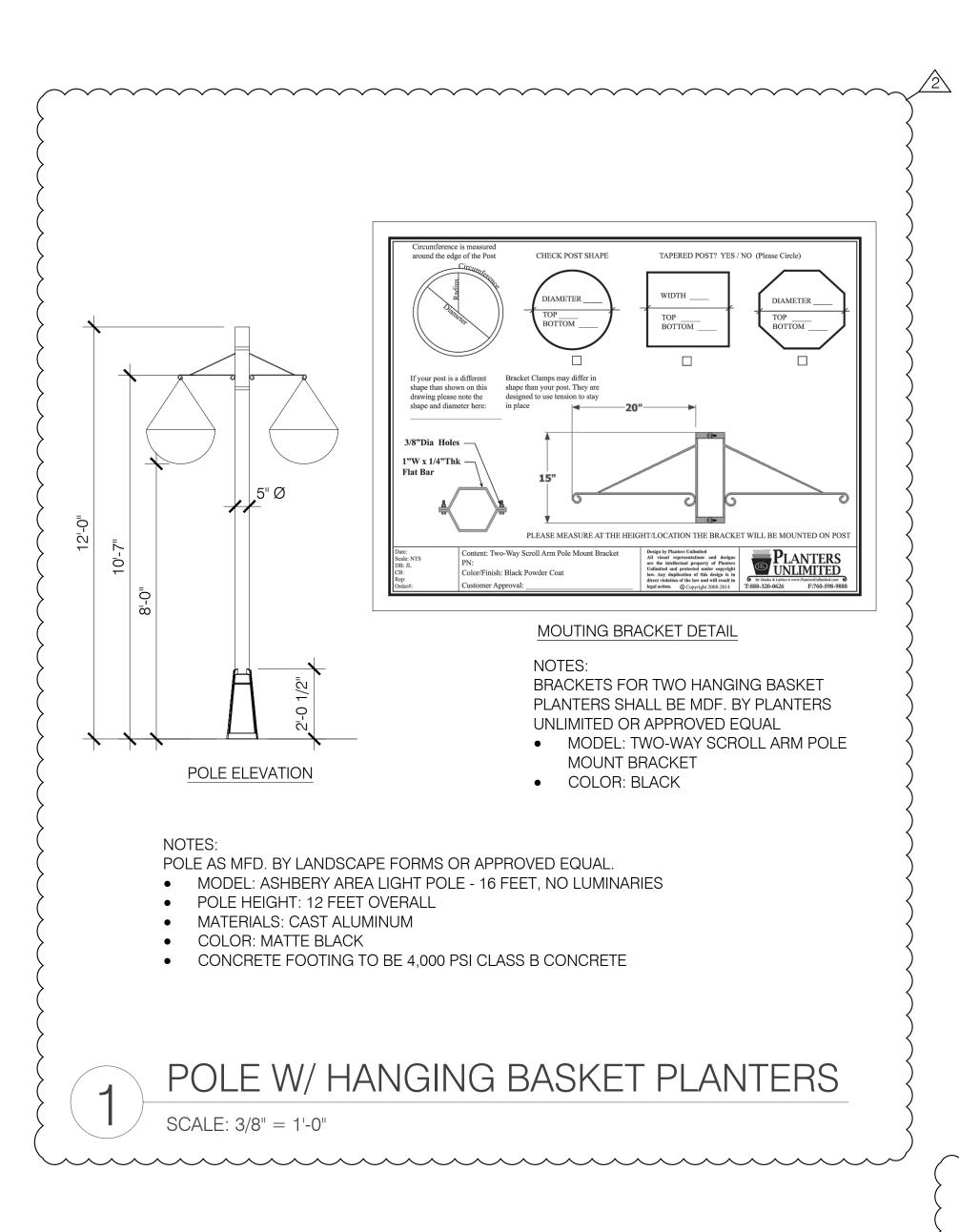


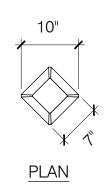
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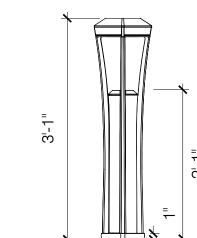
AS NOTED

02-2020-130







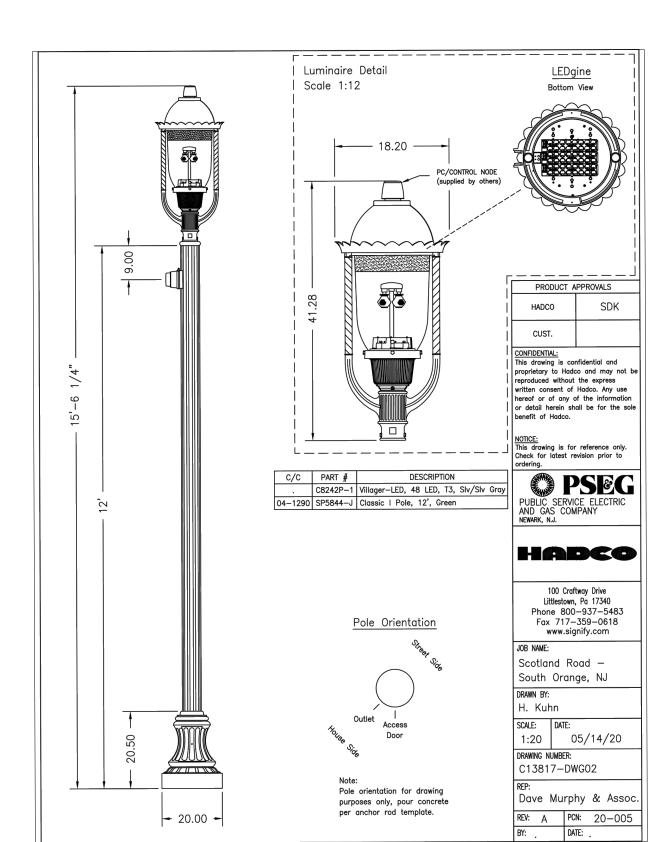


LIGHT BOLLARD AS MFD BY:

- LANDSCAPE FORMS ASHBERY PATH LIGHT
- LED, TYPE 4 DISTRIBUTION, 3000 K, DIMMABLE POWDER COATED COLOR MATTE BLACK
- CONCRETE FOOTING TO BE 4,000 PSI CLASS B
- CONCRETE

LIGHT BOLLARD - TYPE C

SCALE: 3/4" = 1'-0"



STREET LIGHT - TYPE A

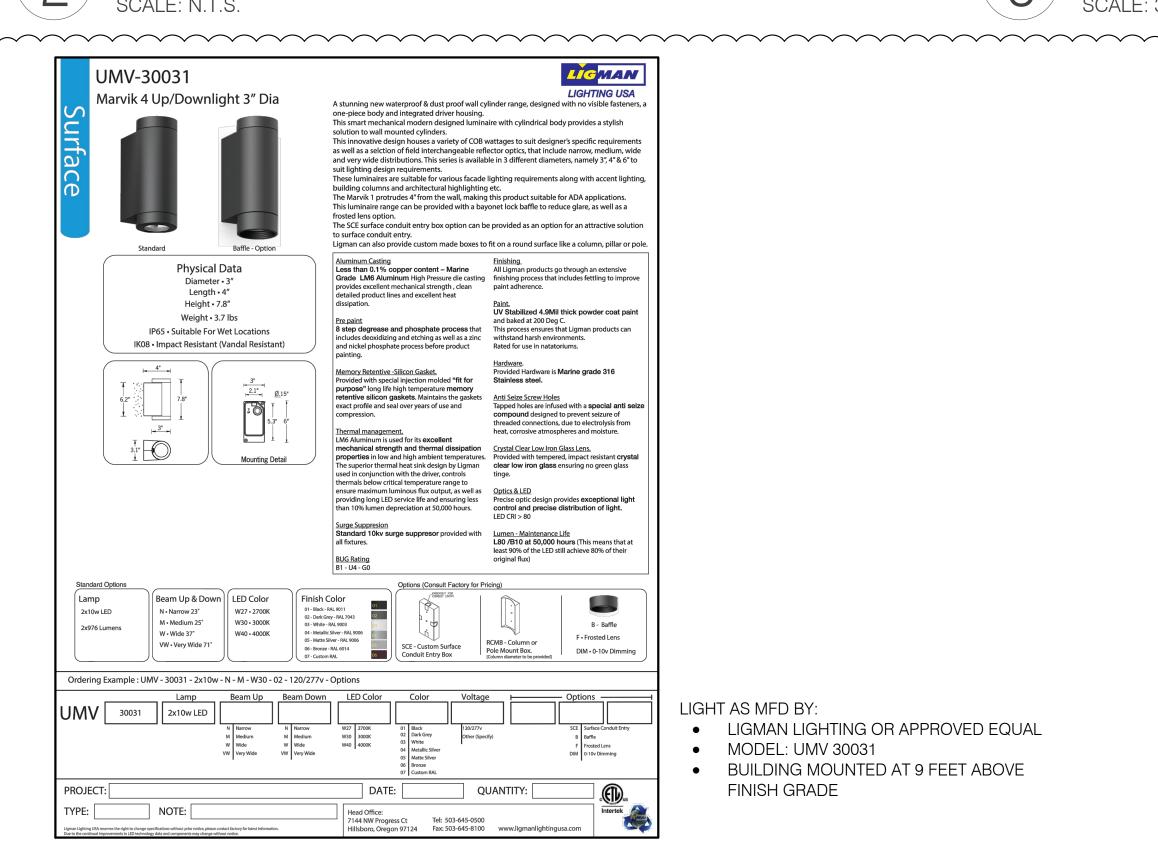
• CONCRETE FOOTING TO BE 4,000 PSI CLASS B CONCRETE \_\_\_\_\_

Ø4" AXON

PEDESTRIAN SCALE LIGHT AS MFD. BY LANDSCAPE FORMS OR

- MODEL: ASHBERY AREA LIGHT 16 FEET, SINGLE LUMINAIRE LED, TYPE 3 DISTRIBUTION, 3000 K, DIMMABLE
- OPTIONS: WITH BRACKETS FOR TWO HANGING BASKET
- PLANTERS & BANNER MATERIALS: CAST ALUMINUM
- COLOR: MATTE BLACK
- CONCRETE FOOTING TO BE 4,000 PSI CLASS B CONCRETE

DECORATIVE LIGHT - TYPE B



LIGHT AS MFD BY: LIGMAN LIGHTING OR APPROVED EQUAL MODEL: UMV 30031 BUILDING MOUNTED AT 9 FEET ABOVE FINISH GRADE

BUILDING MOUNTED LIGHT - TYPE D

\_\_\_\_\_

NOT FOR CONSTRUCTION

TAYLOR & VOSE

DEVELOPMENT

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ar erial

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Landscape Architect

PRELIMINARY AND FINAL SITE

VOSE AVE. APTS. URBAN RENEWAL, L.L.C.

New Jersey Licensed Landscape Architect

Issue: No. Date

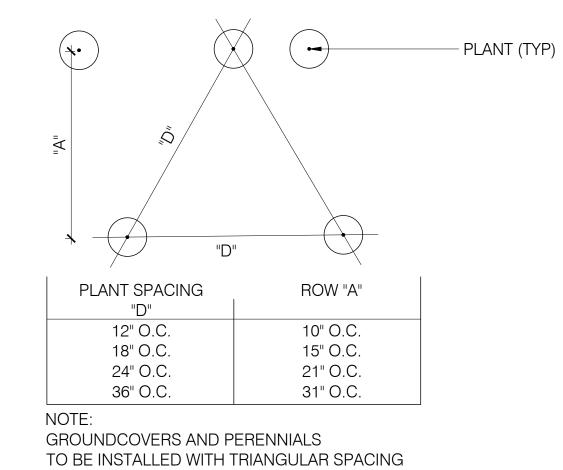
08/06/2020 Updated Preliminary & Final Site Plan Set 08/21/2020 Updated Preliminary & Final Site Plan Set

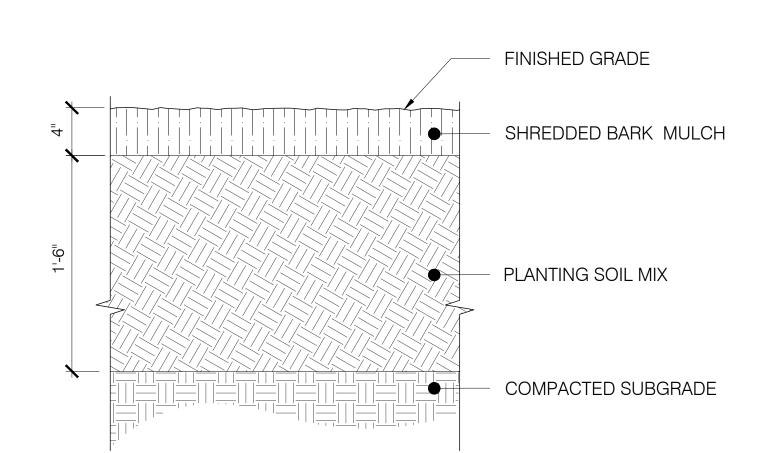
CONSTRUCTION DETAILS L-503 DL 07/07/2020

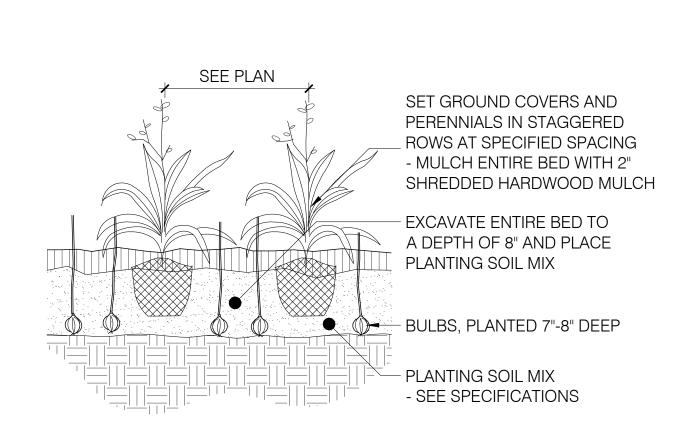
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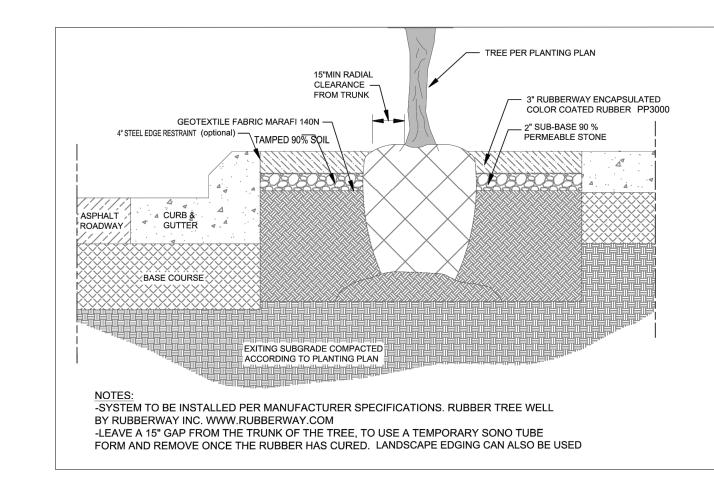
AS NOTED

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- 1. RUBBER TREE WELL SYSTEM SHALL BE PROVIDED BY RUBBERWAY OR
- 2. TYPE: SINGLE LAYER PREMIUM RUBBER SHRED TREE WELL
- 3 COLOR: MAHOGONY
  4. SEE PRODUCT SHEETS PROVIDED FOR MORE INFORMATION

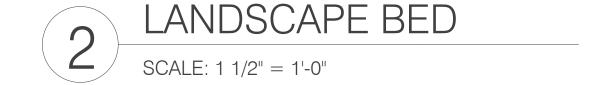
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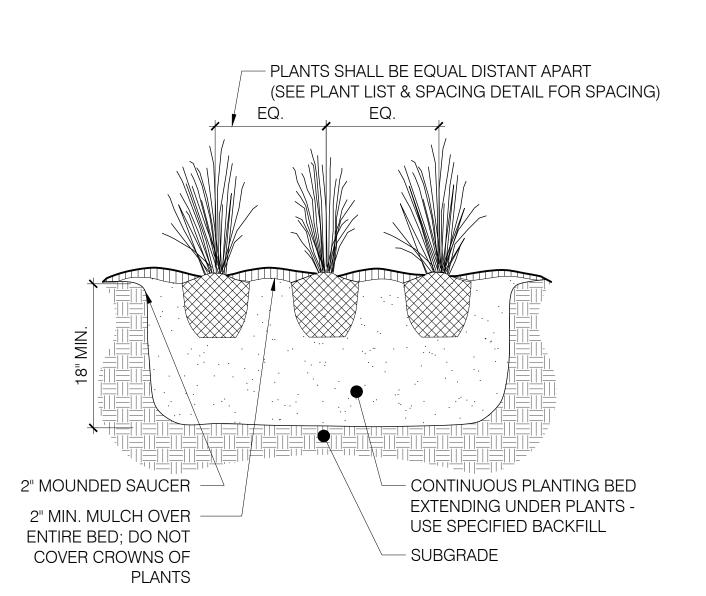
### BONDED RUBBER TREE WELL

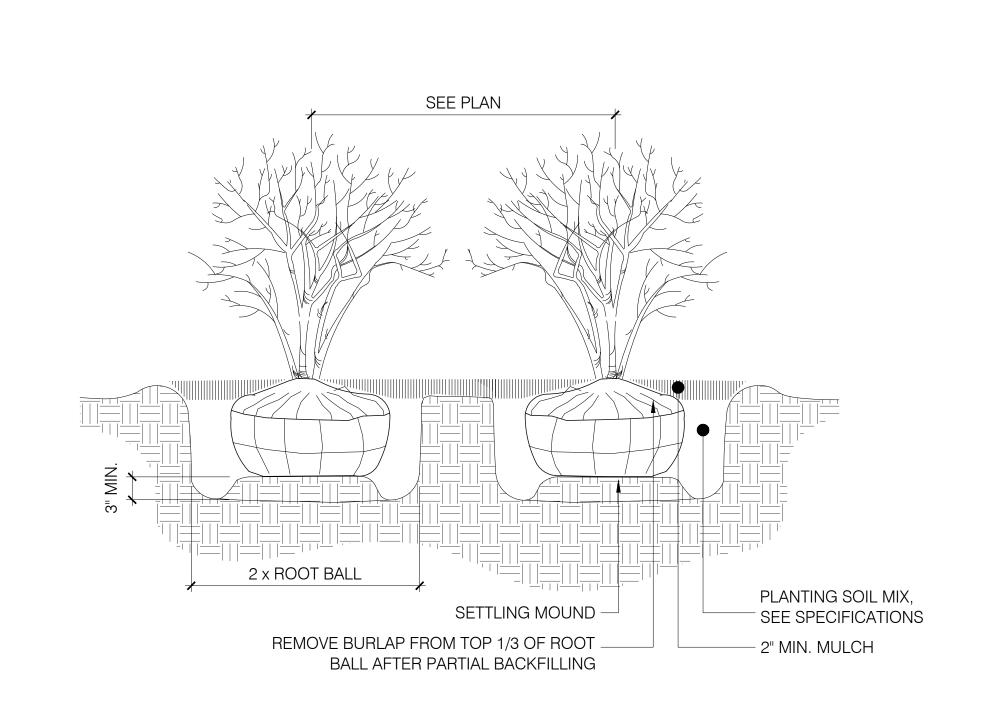
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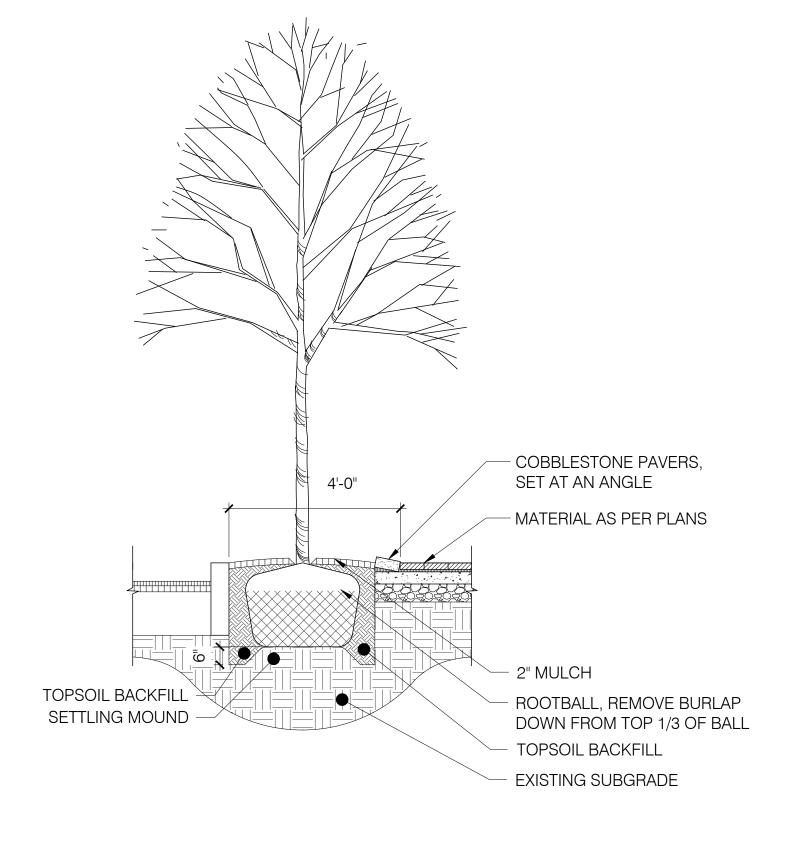




















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AS NOTED

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08/21/2020 Updated Preliminary & Final Site Plan Set

CONSTRUCTION DETAILS DL

L-504 07/07/2020

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